

QUESTIONS LODGED PURSUANT TO STANDING ORDER NO.18 FOR REPLY AT THE MONTHLY MEETING OF DUBLIN CITY COUNCIL TO BE HELD ON MONDAY, 4th OF MARCH 2024

Q.1 COUNCILLOR DERMOT LACEY

To ask the Chief Executive if he will have the following issue and suggestions contained therein examined with a view to increasing safety at Ashfield Road, Ranelagh. In particular if he will examine if the narrowing of the road through extended footpaths could be considered as part of the proposed Cycle Way through Ranelagh.

Due to a delivery service operating in the vicinity there are multiple motorbikes (maybe 20/30) driving the wrong way up Ashfield Road every day without any repercussion. This is extremely dangerous. I know people can breach one-way systems here and there on a bike or by accident but this is systematic due to the delivery service which operates out of the back laneway. Something needs to be done about it as I am certain there will be an accident with someone seriously hurt and this is completely preventable.

Things which could help is narrowing the bottom of the road by broadening the footpath, putting up clearer signs that it's one way, maybe putting up CCTV, maybe having a Garda there on a busy night to put them off. I am genuinely concerned that I'll hear a horrendous crash on my road due to the ignorance of these delivery drivers driving up our road to get down the back laneway. And this is something which everyone is ignoring.

CHIEF EXECUTIVE'S REPLY:

The Transport Advisory Group (TAG) area engineer has conducted an assessment of the traffic situation on Ashfield Road, resulting in the following recommendations:

The road currently lacks road markings indicating the direction of flow. Therefore, it is proposed to install 4 no. RRM 004 Direction Arrows on Ashfield Road (SE). Additionally, the "No Entry" road marking at the exit point of Ashfield Road into Ranelagh Road (R117) will be renewed.

The issue of vehicles traveling in the wrong direction on Ashfield Road has been raised by TAG during the monthly meeting, and it has been brought to the attention of the local Gardaí for necessary action. In future instances, non-compliant vehicles should be reported to the Gardaí for enforcement.



As part of the Sandyford Clonskeagh to Charlemont Street (SC2C) permanent active travel scheme, it is proposed to install priority pedestrian and cyclist access by means of raising the road level to footpath level where Ashfield Road intersects with Ranelagh.

The proposals were presented at public consultation in Q1 2023, and are available on the DCC Active Travel Website (dublincity.ie/activetravel). The preliminary design is being progressed currently, with ongoing engagement with local stakeholders. Construction of the permanent scheme is scheduled to commence in Q2 2025, with an interim scheme in Ranelagh to commence in Q3 2024.

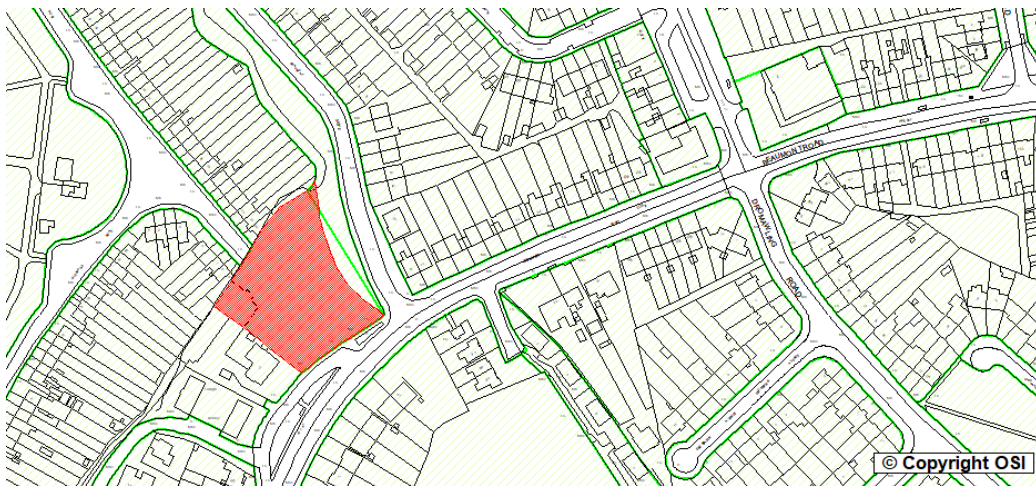
Q.2 COUNCILLOR ALISON GILLILAND

To ask the Chief Executive to arrange for the inspection of **(details supplied)** with a view to determining it a derelict building and entering it in the Derelict Sites Register.

CHIEF EXECUTIVE'S REPLY:

Dublin City Council identified land in scope for the RZLT tax through the publication of draft and supplemental maps in 2022/23. The first annual mapping process is now complete with the publication of the final map on 1st December 2023. The site at **(details supplied)** is included on this final map. In Budget 2023 The Minister for Finance, and the Minister for Housing, Local Government and Heritage, announced the deferral of the initial liability date for the RZLT by one year, from 1 February 2024 until 1 February 2025.

The Annual draft RZLT map for 2024 was published on the Dublin City Council website on 1 February 2024. The site at **(details supplied)** is included on this map. Landowners will have the opportunity to make submissions regarding the inclusion of their land on the map and interested individuals or third parties may also make submissions in respect of land. In December 2024, Local Authorities will publish final maps which will reflect outcomes of all submissions and appeals and any zoning or servicing changes which have taken land out of scope or brought land within the scope of the tax during 2024. This final map will form the basis for the administration of the tax measure by the Revenue Commissioners



Q.3 COUNCILLOR JANE HORGAN-JONES

To ask the Chief Executive the following question with in regards the installation of water pipes at **(details supplied)**

CHIEF EXECUTIVE'S REPLY:

As the issues raised are a matter for Irish Water they should be referred directly to Irish Water.

Please call the telephone number on 0818 178 178 / 0818 578 578 and identify yourself as an Elected Representative. You will be put through to the Local Representative Support Desk in Irish Water's Customer Contact Centre who will deal with you directly. Alternatively, there is a specific e-mail address at Irish Water for Councillors: LocalRepSupport@water.ie.

The Local Representative Support Desk at Irish Water operates from 9.00am – 5.30pm, Monday to Friday.

Q.4 COUNCILLOR JANE HORGAN JONES

To ask the Chief Executive the following question.

There is obviously major disruption going on with the construction of the cycleway at Fairview/Clontarf. However, I wish to highlight that there is increasing danger for pedestrians coming on-stream at the Malahide Road/Marino Crescent and Clontarf Road junctions. There is an 'island' there with a popular bus stop. There is a pedestrian light crossing to and from Fairview to the 'island'. However, there is no safe crossing at the Malahide Road/Marino Crescent junction for pedestrians to get to and from the 'island'. There is also no safe crossing for pedestrians at the Clontarf Road/Marino Crescent slip road junction.

There is extensive traffic now taking the slip road at Marino Crescent not just from the Malahide Road but also from St Aidan's Park Road (cars avoiding the Fairview cycleway). Most of the day there is a build-up of traffic on the slipway.

I am aware that there has been at least one elderly person injured at this location.

Can Dublin City traffic engineers please re-examine the two junctions referred to and put in at least ramps and pedestrian zebra crossings at the locations highlighted.

CHIEF EXECUTIVE'S REPLY:

The Clontarf to City Centre Scheme continues to review the crossing points at the bottom of Malahide Road and the junctions with Marino Crescent and Clontarf Road. The crossing proposals need to take account of a number of elements when considering how these locations are to be managed. The route from Malahide Road and the left turn onto the slip lane to Marino Crescent and to Clontarf Road is (i) a HGV route (ii) an abnormal loads route and (iii) potentially a bus route in the future.

Please see sketch below which identifies the 3no. Crossing locations at Malahide Road/Marino Crescent and Clontarf Road. Please also see below for relevant bullet points to try summarise the current proposals for each of the 3no. crossing locations;

1. Crossing no. 1 – Malahide Road onto slip road at Marino Crescent Crossing;

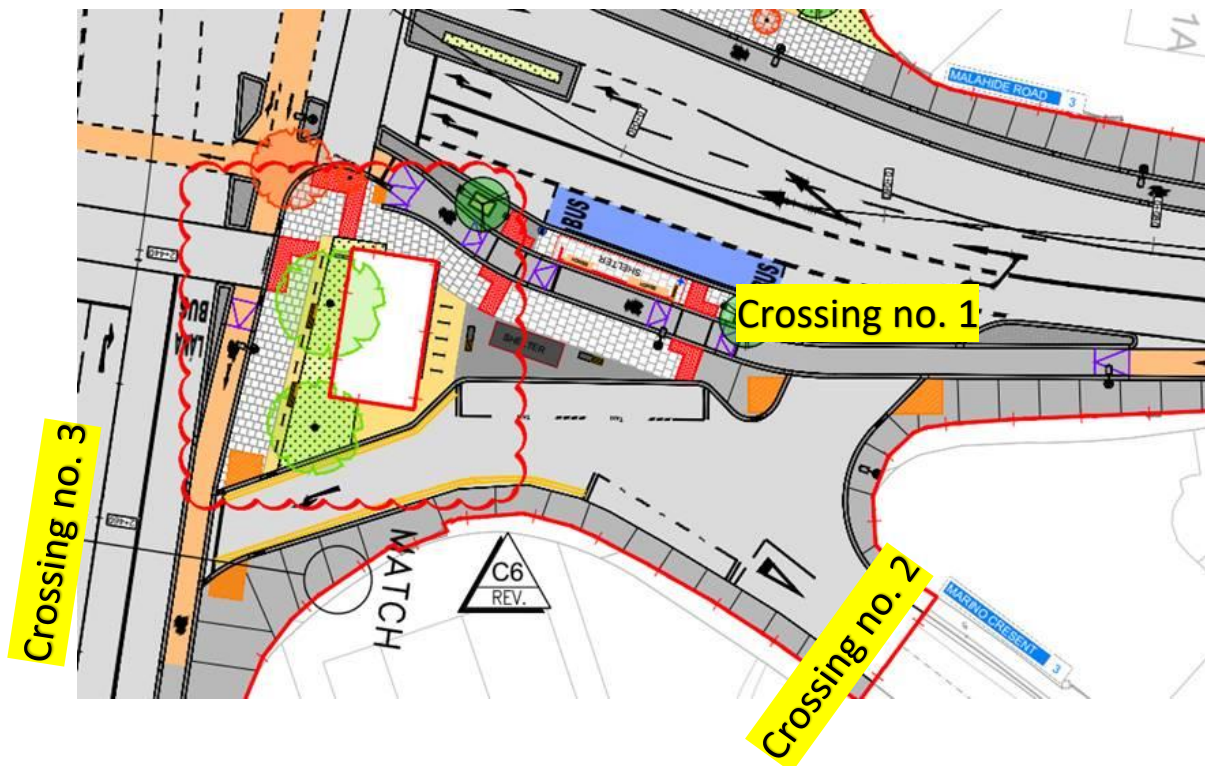
- Current design proposal indicates an uncontrolled pedestrian crossing
- It is not believed that this crossing point is suitable for any form of controlled crossing due to the HGV and bus movements expected from Malahide Road and onto Marino Crescent and the proximity of the Malahide and Clontarf Road junction. The proposed design layout is essentially the same as existed previously with the bus stop also in the same location as was the case previously.
- A raised table which is utilised on side-roads elsewhere along the Scheme is not deemed to be appropriate at this location due to the anticipated traffic expected to utilise the route.

- A raised kerb on each side of the cycle-track coming down Malahide Road is to be constructed with the intention of slowing traffic which intends to make the left-turn onto the slip road.
- At present no changes to the proposed layout are planned for this crossing

2. Crossing no. 3 – Marino Crescent/Clontarf Road:

- Current design proposal has provided for an uncontrolled pedestrian crossing
- It is noted that this crossing point facilitates abnormal loads travelling in both directions (escorted loads).
- A raised table is not deemed to be appropriate due to the HGV and bus movements expected down Malahide Road and onto the slip to exit onto Clontarf Road.
- A raised kerb on the Clontarf Road outbound cycle-track is to be constructed which is intended to slow traffic making the left-turn from the slip-road.
- A further design review shall be undertaken at this crossing point to see if there is any form of controlled crossing which can be incorporated at this location. The Project Team can report once this review has been undertaken.

In summary, we shall endeavour to provide a raised table at Crossing no. 2 (as per below at Marino Crescent) and conduct a further design review of Crossing no. 3. With regard to Crossing no. 1 it is for now intended to leave the arrangement as per the current design which is similar to what existed previously prior to the Scheme being undertaken.



Q.5 COUNCILLOR ALISON GILLILAND

To ask the Chief Executive to arrange for a review of the traffic light sequence at the Griffith Ave/Swords Rd junction, in particular the duration of the green light for vehicles travelling through the junction east/west (Collins Ave - Griffith Ave).

CHIEF EXECUTIVE'S REPLY:

At the junction of Swords Road/ Griffith Ave, the Swords Rd approaches receive a majority of available green time each cycle of the traffic signals as the Swords Road is a main public transport artery to/from the city and the main city transportation route to Dublin airport, Northern Ireland and northern commuter areas.

Allocation of extra green time to Griffith Ave approaches reduces the green time available to the Swords Road and thus increase public transport journey times, delaying hundreds of passengers, contrary to Dublin City Council policy to prioritise sustainable modes of transportation.

Every effort is made to keep traffic moving on all routes in so far as possible, however increasing volume of traffic, coupled with the need to service priority demands from pedestrians, cyclists and public transport means that a balance needs to be struck between these multiple modes, on multiple approaches to junctions.

It is therefore not always possible to prioritise one route over others.

However, the matter has been referred to Brendan O'Brien Head of Technical Services (Traffic) and he is arranging to have an in depth traffic survey carried out at this location to determine the current situation.

Once this has been undertaken it will be assessed to determine if there are any changes that could be made to allow for the situation on Griffith Avenue to be ameliorated while keeping the main public transport corridor working and protecting the safety of pedestrians and cyclists.

We will also continue to monitor the junction in question and amendments to traffic signal timings will be made as required, where there is scope to do so.

Q.6 COUNCILLOR DERMOT LACEY

To ask the Chief Executive the management arrangements for purchase of the new Columbarium Walls at the Donnybrook Cemetery and the overall management of the newly restored facility.

CHIEF EXECUTIVE'S REPLY:

The Columbarium Walls project has commenced on site construction with an intention to have it ready for leasing in Q4 2024. The detailed management of the new facility has not been finalised at present, this information will be brought to the Area Committee when available.

Q.7 COUNCILLOR JANE HORGAN-JONES

To ask the Chief Executive to fix the path on Melrose Avenue urgently as it is very dangerous and people will fall. It's broken from the tree. **(Details supplied).**

CHIEF EXECUTIVE'S REPLY:

An enquiry (ref. 11153702) has been created and forward to the Area Inspector who will carry out a survey as soon as possible given the circumstances & arrange for any necessary repairs.

Q.8 COUNCILLOR JANE HORGAN-JONES

To ask the Chief Executive again about the illegal parking on Brookwood Rise/Harmonstown Road. People are parking on the paths still and it's very dangerous to pedestrians which many are very elderly. I have had several residents contact me, something needs to be done. The speed Ramps need to be fixed also they are broken.

CHIEF EXECUTIVE'S REPLY:

Parking Enforcement Part Reply:

Dublin Street Parking Services, our parking enforcement contractor, have monitored and enforced this area over the last number of weeks. They will continue to patrol and enforce this area. I have asked the Area Engineer to assess if parking restrictions can be implemented here to deter illegal parking.

Road Maintenance Services Reply;

The request for repairs to the defective ramps Brookwood Rise/Harmonstown Road has been forwarded to the Area Inspector who will assign a crew to carry out temporary repairs.

These ramps will be added to subsequent works programmes for full resurfacing.

Q.9 COUNCILLOR CHRISTY BURKE

To ask the Chief Executive at what stage are the discussions with Dublin City Council the HSE and government ministers are at regards the €17 million owed to Dublin City Council for ambulance services to HSE over a number of years.

CHIEF EXECUTIVE'S REPLY:

Department of Health and the Department of Housing, Local Government and Heritage are developing a joint memo to Government outlining the arrangements and seeking approval to implement the Task & Finish Group's recommendations and to provide the necessary funding.

CE has yet to see the actual contents of the memo but believes it does take account of the work undertaken by the Task and Finish Group.

Q.10 COUNCILLOR CHRISTY BURKE

To ask the Chief Executive for an update on what stage funding has been approved regards (**details supplied**).

CHIEF EXECUTIVE'S REPLY:

Stage 1 project and funding approval was received from the Department of Housing, Local Government and Heritage in January 2024 for the regeneration of (**details supplied**).

The City Architects Division are now appointing a consultant led design team in order to develop the plans for the project. The approach for redeveloping the existing housing scheme is to retain the buildings and carry out a deep retrofit to provide quality new homes.

Surveys are ongoing onsite and it is envisaged that the Part 8 process will commence in Q4 2024, ahead of planning lodgement in Q2 2025.

Q.11 COUNCILLOR CHRISTY BURKE

To ask the Chief Executive for details of the number of applicants on the housing list with Dublin City Council

CHIEF EXECUTIVE'S REPLY:

Please find attached the latest housing report setting out the figures requested.

Q.12 COUNCILLOR CHRISTY BURKE

To ask the Chief Executive when will the new Dog Warden staff be in place and on duty in the City Dublin?

CHIEF EXECUTIVE'S REPLY:

The Council are required to provide one or more persons to provide dog warden services under Section 15(1) of the Control of Dogs Act 1986

15.—(1) Every local authority shall employ one or more persons to be dog wardens for the purposes of this Act and every person so employed shall be furnished with a certificate of his appointment and when exercising a power under this Act shall, if requested by any person thereby affected, produce such certificate for inspection by the person.

To this end Dublin City Council has completed a successful recruitment process for Animal Wardens (to cover both Control of Dogs and Control of Horses services). Offers have been made to 5 successful candidates. The first 2 wardens have taken up their posts with effect from the 12th February 2024. A further 2 wardens will take up duty on the 26th February and the remaining post will be filled at a later date. This will bring the full staffing compliment of Animal Welfare & Control Services up to 12.

All new Wardens will follow an extensive education programme to include dog and horse handling skills, health and safety training and familiarity with animal welfare legislation. The Council will continue to contract an external resource to provide warden services until the new wardens are fully trained.

Q.13 COUNCILLOR DECLAN FLANAGAN

To ask the Chief Executive how much community funding has Dublin City Council given the Artane Beaumont Family Recreation Centre over the past 5 years? Who owns the community centre, can you provide background to same.

CHIEF EXECUTIVE'S REPLY:

Community and Social Development Office would use the centre for hire of the hall. From 2018-2024 the section has paid €5,300 for room hire. Discretionary budget funded the upgrade of a door, totalling €5,170 in 2019. Artane Beaumont Recreation Centre did not receive community grants through our central fund from 2018-2023.

Q.14 COUNCILLOR DECLAN FLANAGAN

To ask the Chief Executive what is the update on Parnells Football Club and Club house. Has Dublin City Council had any meetings with the club over the past two years, the club is in financial debt?

CHIEF EXECUTIVE'S REPLY:

The Area Manager has met with Parnell's football club in the past two years however no discussion was brought up with regards to financial debt.

Q.15 COUNCILLOR DECLAN FLANAGAN

To ask the Chief Executive can the continuous white line outside (**Details supplied**) be removed ASAP please? Why was this continuous white line placed outside the houses when the occupants have to park their vehicles there, why are Dublin City Council placing fines on the cars here?

CHIEF EXECUTIVE'S REPLY:

The request for rescinding the continuous white line (Enquiry Ref: 7037593) will be referred to the Area Engineer for assessment and report to the Transport Advisory Group for their consideration. The Councillor will be informed of the final recommendations of the Transport Advisory Group in due course.

Q.16 COUNCILLOR DECLAN FLNAGAN

To ask the chief executive is there new parking enforcement personnel operating around the Coolock area, if so can you advise on the background to this please.

CHIEF EXECUTIVE'S REPLY:

Dublin Street Parking Services, the parking enforcement contractor, patrol and carry out enforcement in the administrative area of Dublin City Council.

Q.17 COUNCILLOR MAIRE DEVINE

To ask the Chief Executive for the details of the "Legal Warrantees" that the Council agrees with and signs contracts for with tendered construction firms of social/public housing – either new or refurbished units? Can this be provided in a tabulated format?

CHIEF EXECUTIVE'S REPLY:

For homes using the Direct Delivery Route (where DCC enters into a building contract with a builder), we use the forms of Contract for Public Works which are published by the Office of Government Procurement. Following substantial completion of direct delivery housing schemes, defects that may arise are dealt with under Clause 8. Quality, Testing and Defects.

Substantial Completion in practical terms is when the completed building is placed on the Building Control Management System Register and the contract administrator, the Employers Representative (ER) confirms the building has reached substantial completion and the homes are then available for people to move into.

There is a Defects Period starting on the date of substantial completion of the works and continuing for the period stated in the Schedule (which is generally set at 12 months). Where defects arise during this period the ER brings them to the attention of the Contractor and DCC, and subsequently the defects are addressed.

DCC Contracts are normally under seal and therefore the parties have rights for 12 years.

If a defect becomes apparent after the Defects Period, and there are no contributing causes that may have impacted post occupancy, DCC would engage with the Contractor to examine the cause of the defect, review the possible solutions and agree remedial actions.

If the Councillor has any specific housing scheme in mind or would like further information, I am available at any time to discuss.

Q.18 COUNCILLOR LORCAN O'TOOLE

To ask the Chief Executive that the contractors refrain from leaving 'grass cuttings' on public footpaths after mowing as it causing a danger to pedestrians.

CHIEF EXECUTIVE'S REPLY:

The Parks Service has issued an instruction that all arisings from their grass cutting activities be removed after they are finished in the respective areas.

Q.19 COUNCILLOR RAY MCADAM

To ask the Chief Executive to ensure that the damaged footpath outside (**Details Attached**) is repaired immediately; and if he will make a statement on the matter.

CHIEF EXECUTIVE'S REPLY:

An enquiry (Ref: 11153988) has been created and forwarded directly to the area inspector to assess the location and carry out any necessary repairs as soon as possible.

Q.20 COUNCILLOR DERMOT LACEY

To ask the Chief Executive if he will give a comprehensive reply to the issues contained in the letter (**details supplied**) with this Question relating to the issuing of (Residential) Disc Parking permits for foreign registered cars.

CHIEF EXECUTIVE'S REPLY:

The Dublin City Council Parking Control Bye Laws 2024 govern the issuing of Residential Parking Permits. There are certain criteria that must be met in order to be eligible to apply for a permit (eg: type of property, quota available at property, if applicant is a resident etc). An applicant is also requested to submit an application form where they declare that they reside at the property for at least 183 days per year, two proofs of residency, an Insurance Certificate at the address in which they reside and are applying for the permit and a copy of their Drivers Licence. Our remit is in relation to the issuing of a permit to a resident and whether or not a person is eligible to apply for a permit and, if they are, to ensure the correct supporting documentation is submitted with the application. We don't have the authority or the right under GDPR Guidelines to request any other type of information that doesn't relate to the issuing of a permit directly. Whether the vehicle is registered in Ireland or outside Ireland is of no significance to the eligibility of obtaining a permit.

Q.21 COUNCILLOR DEIRDRE CONROY

To ask the Chief Executive and Roads Department to investigate the safety along the road to the Harold's Cross Educate Together children's school. I was notified by a school enquirer that there are several serious road safety issues on the way into Harold's Cross Educate Together National School, which pose a risk to the local school children. I request that school traffic wardens be installed at all three of these points along the school route before an accident involving a child happens. Also that suitable signage is placed in these three areas. Many of the children who come from the Mount Argus/Lower Kimmage Rd area take this route daily. Please note that there is no alternative route for a lot of children travelling from the Mount Argus/Lower Kimmage Rd area. Many children are in Junior Infants and 1st Class and their journey into school would be fairly typical of children using this route.

CHIEF EXECUTIVE'S REPLY:

A request for a school warden to assist children will be assessed and referred to the Traffic Advisory Group for examination. The Councillor will be informed of the recommendation in due course.

The Transport Advisory Group (TAG) area engineer have conducted a preliminary assessment of the traffic safety issues presented at the junctions of Harold's Cross Road (SE) / Mount Argus Road (SE), and Harold's Cross Road (SE) / Parkview Avenue (SE). Based on the TAG preliminary assessment, please see the following recommendations:

Location 1 – Junction of HAROLD'S CROSS ROAD (SE) / MOUNT ARGUS ROAD (SE)

1. "Dangerous Road Situation 1" speed of cyclists and breaking the traffic light during green time for the pedestrians, further on site assessment will be conducted by the TAG area engineer, and the Councillor will be informed of the recommendation in due course.
2. The junction of Harold's Cross Road (SE) / Mount Argus Road (SE) including the pedestrian crossing, is subject to some upgrade works as part of the BusConnects Kimmage to City Centre route as shown in the screen shot below.
 - a. For more information, please refer to page 10 of the General Arrangement Drawings of BusConnects Kimmage to City Centre route at <https://kimmagescheme.ie/>.

Location 2 – Junction of HAROLD'S CROSS ROAD (SE) / PARKVIEW AVENUE (SE)

1. "Dangerous Road Situation 2" relates to the pedestrian crossing layout, speeding, sudden take off and driving over the pavement. Further on site assessment will be conducted by the TAG area engineer, and the Councillor will be informed of the recommendation in due course.
2. The pedestrian crossing at both arms of the junction is subject to some upgrade works as part of the BusConnects Kimmage to City Centre route as shown in the screen shot below.
 - a. For more information, please refer to page 10 of the General Arrangement Drawings of BusConnects Kimmage to City Centre route at <https://kimmagescheme.ie/>.
3. "Dangerous Road Situation 3" relates to the cyclists and cars breaking red light when turning right (outbound) on Harold's Cross road, and the conditions of the traffic sign posts at this junction. The TAG will conduct an onsite assessment for the renewal of existing road signs and road markings on the road.

Q.22 COUNCILLOR DERMOT LACEY

To ask the Chief Executive to report on the current status on the move to a 30 KPH speed limit across Dublin City and if it is possible, or in traffic terms desirable, to move to such a limit on the stretch of the N11 from Nutley Lane to Donnybrook Church and through Donnybrook. In particular what possibilities exist to introduce sensible traffic controls in tandem with the Bus Connects project throughout the City and specifically the stretch of road mentioned above in this question.

CHIEF EXECUTIVE'S REPLY:

Dublin City Council Road Safety Strategy Action No 8 explains that 30 Kph is the speed limit in all areas in Dublin City except arterial routes. The remaining routes will be examined when the guidelines for setting speed limits have been updated.

Q.23 COUNCILLOR LARRY O'TOOLE

To ask the Chief Executive to repair and restore and make safe the paving/pathway around the MUGA/Green on Bunnary Road.

CHIEF EXECUTIVE'S REPLY:

An enquiry (ref. 11154151) was created and forwarded to the Area Inspector, who will carry out a site inspection and assign any footpath defects to a crew for repair.

Q.24 COUNCILLOR LARRY O'TOOLE

To ask the Chief Executive To conduct a full weed removal and road cleaning of Bunratty Road inclusive of cul de sac areas.

CHIEF EXECUTIVE'S REPLY:

The weed spraying contract does not start until early April and the public domain team can include Bunratty Road on the list of roads to be sprayed in 2024. Waste Management Services had Bunratty Road, inclusive of cul de sac areas, cleaned on the 22nd February 2024.

Q.25 COUNCILLOR MÁIRE DEVINE

To ask the Chief Executive to seek interim funding from the Department for significant “meantime” refurbishment of the Tyrone Place D8 flat complex ahead of any plans for its future.”

CHIEF EXECUTIVE'S REPLY:

As the Regeneration Programme Manager I will liaise with Housing Maintenance Management to understand options that may be available in the interim and to examine if Tyrone Place could be considered for future Enhancement Work programmes.

Q.26 COUNCILLOR MÁIRE DEVINE

To ask the Chief Executive to organise a site visit to Tyrone Place D8 to discuss with the family resource centre managers the numerous issues, in particular the significant damp and mould, that residents are enduring which is proving detrimental to their well-being.

CHIEF EXECUTIVE'S REPLY

Housing Maintenance Engineering Section will arrange to meet with the management of the Family Resource Centre in Tyrone Place to discuss the above issues.

Q.27 COUNCILLOR LARRY O'TOOLE

To ask the Chief Executive to make provision for pedestrian crossing facilities at (details supplied).

CHIEF EXECUTIVE'S REPLY:

This request (Enquiry Ref: 7037769) will be referred to the Area Engineer for assessment and report to the Transport Advisory Group for their consideration. The Councillor will then be informed of the final recommendations of the Transport Advisory Group.

Q.28 COUNCILLOR MÁIRE DEVINE

To ask the Chief Executive for an update with regards to the request by Sporting Liberties & Donore Project Consultative Forum that resources be ring-fenced for the playing pitch at St Teresa's Gardens , Dublin 8 that protect the project from future economic downturns or known unknowns?”

CHIEF EXECUTIVE'S REPLY:

DCC and the LDA are partnering in developing a mixed use scheme of 543 homes on the site of the Former St. Teresa's Gardens lands. Planning was granted for this development in June 2023. Contractor procurement is underway and construction will commence late this year. The Development will be delivered in phases up to the

beginning of 2028. As part of these works DCC are developing a new municipal pitch, changing rooms and boxing club.

A planning application was submitted to An Bord Pleanála for the municipal pitch, changing rooms and boxing club last year. A decision on this was due to be determined by November 2023. However no decision has been made to date and there is no indication of date for such decision.

In lieu of this, DCC intend on submitting a planning application for the development of Municipal pitch and changing rooms through a Part 8 Planning route. Once planning is granted DCC will jointly develop the pitch and changing rooms with the LDA as part of the overall delivery of the mixed use scheme outlined above.

At present DCC are in ongoing correspondence with Sporting Liberties defining their requirements for the facilities.

DCC with the DHPLG are committed to the delivery of this scheme including the pitch facilities.

Q.29 COUNCILLOR DECLAN MEENAGH

To ask the Chief Executive to help get medical priority for **(details supplied)**.

CHIEF EXECUTIVE'S REPLY:

The above applicant is on the Housing Transfer List, with an application date of 16/01/2019, the applicant holds the following position on this list;

Area	Bedsizes	Position
N	1	378
P	1	210

Dublin City Council allocates properties based on time on the list and currently there are applicants on the waiting list of longer standing who have to be considered as suitable vacancies arise. It is not possible to indicate accurately the prospect of an offer for any applicant. The number of vacancies arising will be a determining factor in the length of time applicants could expect to be on the waiting list.

I note from the applicant's file that he has recently submitted a medical application, a decision on this is expected soon. The applicant will be notified of the decision.

Dublin City Council are increasing the number of units that are being allocated through the Choice Based Letting Scheme, such vacancies are advertised on the DCC website. If there are no properties available at present, nothing will be advertised. Applicants can only apply for properties in their chosen areas that meet the size requirement of their household.

The applicant should continue to liaise with Housing Maintenance regarding the concerns surrounding the safety issue of the property.

Q.30 COUNCILLOR DECLAN MEENAGH

To ask the Chief Executive the following:

To ask the DRHE to provide, in tabular form, a breakdown of the number of beds by room type i.e. 1 bed per room, 2 bed per room, 3 bed per room, for all room configurations, and to provide these figures over the past 5 years.

CHIEF EXECUTIVE'S REPLY:

The DRHE does not keep records in this format. Room configurations can change depending on factors like need, family size and accessibility requirements. Additionally, room configurations were changed in the years during the pandemic, in response to social distancing rules.

Q.31 COUNCILLOR DECLAN MEENAGH

To ask the Chief Executive the following:

To ask the DRHE for the number of hours provided in support and key worker services in privately operated homeless hostels and to provide the amount of hours and services offered.

CHIEF EXECUTIVE'S REPLY:

All privately operated homeless hostels (singles and families) are provided support from the Housing Support Officer (HSO) teams in Parkgate Hall. This support amongst other things takes the form of regular clinics in each accommodation. HSO's work with residents to ensure their housing files are up to date and work with them to identify suitable exits from homelessness through Homeless HAP, LTA supported accommodation, CBLS's etc. Contact details for each housing Support officer are prominently displayed in each service they are linked to so that every resident access to a HSO. In the case of singles accommodation

The PACT team provide additional supports across all PEA's and work closely with the HSO team around housing but also provide more holistic supports around health/addiction supports etc. Every person is given an initial assessment within the first 4 days of entering accommodation for the first time to identify these support needs. To date the team have carried out 3,416 initial assessments, and 12,297 visits and have case managed 2,222 individuals, with 1,185 cases currently opened.

In the case of families the majority of private family Hubs have Focus Ireland support workers attached but for the ones that don't, where additional support is required the HSO team will refer the family to the Focus Ireland family Centre team for additional visiting supports. If the only support needs are around housing these families will still have a HSO linked to them on an ongoing basis to support around exiting homelessness.

Q.32 COUNCILLOR DECLAN MEENAGH

To ask the Chief Executive if he will re-visit providing on-street charging options for people living in areas without gardens. Does the manager accept that the lack of on street charging points is a barrier for people in buying EVs and will he do something to encourage more EVs in areas like Phibsborough with limited or no off street parking?

CHIEF EXECUTIVE'S REPLY:

The DLAs are currently evaluating tenders received for a pilot project to provide public EV charging facilities across the County. Dublin City Council has identified 14 locations suitable for fast charging hubs. The pilot project will be rolled out in 2024. Based on the findings of the pilot project we may consider additional locations for fast charging hubs.

The Environment & Transportation Department accepts that the preferred method of EV charging for many owners is from a domestic supply. Unfortunately it is not always possible to install a domestic EV charger as many properties and neighbourhoods are unsuitable.

The City Council's transport strategy prioritises active travel (walking and cycling) as the most sustainable mode of transport for the future of Dublin City. Public transport is the next preferred mode of transport. The City Development Plan and Draft Transport Plan aim to increase walking, cycling and public transport use by 2028.

Q.33 COUNCILLOR JOHN LYONS

To ask the Chief Executive when the Oscar Traynor Road Consultative Forum will be re-established, when the Oscar Traynor Road Community Fund will be established, how will funding decisions be made and by whom and what is the defined area within which Community Fund monies can be allocated.

CHIEF EXECUTIVE'S REPLY:

Enabling works commenced on site just before the end of last year. These works are ongoing and expected to continue to mid-year. The Main contract commences in April next and as such we intend re-establishing the Consultative Forum to align with this timing.

The Area Office are currently working on governance and oversight proposals for the Oscar Traynor Community Gain Capital Fund. We are also working on a Terms of Reference for the proposed Liaison Committee and the establishment of a community audit that could guide the distribution and allocation of funding in the future. We will bring these proposals to Local Councillors over the coming weeks.

Q.34 COUNCILLOR VINCENT JACKSON

To ask the Chief Executive look into the possibility of developing bring centres that would allow persons using the facilities for personal use remove and reuse, timber, cycles, furniture etc. could be easily re-used by others. Can I ask what currently happens to goods left in bring centres. Is anything re-used? Can management indicate will Ballyfermot ever get a proper bring centre.

CHIEF EXECUTIVE'S REPLY:

Dublin City Council's Civic Amenity Sites and Bring Centres operate under Certificates of Registration (CoRs) issued by the Environmental Protection Agency. CoRs currently prohibit any scavenging of materials from these sites. This may however change in the future as objectives relating to a focus on re-use have been included in the forthcoming National Waste Management Plan for A Circular Economy.

There is a variety of ways items left in bring centres and Civic Amenity Sites are dealt with, namely preparation for re-use, recycling and recovery - depending on the particular stream. High value clothing and textiles are typically re-sold for re-use on international markets, while damaged or lower value goods are repurposed as furniture stuffing and/or industrial cleaning rags. Garden waste is recovered via an industrial composting procedure. Glass bottles are crushed and ultimately recycled as new bottles. WEEE items are typically stripped to their component parts and recovered for a variety of uses, depending on the materials extracted, as is furniture and other bulky waste items. Any materials that cannot be prepared for reuse are typically recovered as Solid Recovered Fuel (SRF) for use in the cement making industry.

While there are currently no plans to develop an additional or alternative bring centre in Ballyfermot, Waste Management Services intend to undertake a study this year to assess our current recycling infrastructure and to develop a strategy for the future development of such facilities in line with the aforementioned national waste plan, projected population increase and relevant land use zoning.

Q.35 COUNCILLOR VINCENT JACKSON

To ask the Chief Executive to once again please look at why the left turn light was removed from Grattan Crescent Inchicore turning onto Emmet Road at the Black Loin Public House there is no logic in containing traffic for long periods and condemning motorists, buses etc to long delays whilst waiting for traffic lights to change. Traffic travelling towards the City can now take 20 minutes from Sarsfield Road Ballyfermot to the Black Loin turn. The same T-junction operates all over the City and Country without any problems?

CHIEF EXECUTIVE'S REPLY:

Following on from a number of queries from local vulnerable road users in relation to near misses with traffic, the signal operation of this site was changed in February 2021 to remove the left turning filter from operating at the same time as traffic is exiting from Emmet Road, as a large number of vehicles were still trying to pass through the junction when the pedestrian crossing was operating even with a delay on the pedestrian crossing. This had to be rectified due to safety concerns.

In order to help traffic after this change the time allocated to the left turn and straight movement from Grattan Crescent was increased. There are currently no plans to change the operation of this site.

Q.36 COUNCILLOR VINCENT JACKSON

To ask the Chief Executive contact the NTA as a matter of urgency to impress upon them the need to hold proper updated consultation / information sessions with the people of Ballyfermot in relation to the proposed changes to the road network with Bus Connects. The element of confusion with the poor scale of maps and what is proposed is shocking. The last thing the people of Ballyfermot want is a Bus system that forgets about the existing community. The loss of Park lands, valuable space along Ballyfermot Road and hugely unpopular decision to remove the Iconic Assumption Roundabout at the Church of the Assumption Ballyfermot Road needs to be discussed with the local community as this Roundabout currently works very well and relieves pressure on our Roads.

CHIEF EXECUTIVE'S REPLY:

The NTA applied to An Bord Pleanála in July of 2023 for approval of the Scheme under Section 51(2) of the Roads Act 1993 (as amended), along with a submission for confirmation of the associated Compulsory Purchase Order.

An Bord Pleanála has now issued its decisions, being to approve the Scheme, subject to conditions, and to confirm the Compulsory Purchase Order (CPO).

Following An Bord Pleanála's determinations, various public notices and notifications are required to be issued and this will be published/issued at the appropriate time.

Subject to completion of various processes, the NTA will now be developing a construction sequence taking account of any planning consents received, along with documentation to allow procurement to commence for the construction stage of the Scheme.

It is expected that all twelve corridors will be completed in 2030 with the first construction contracts to be awarded at the end of 2024 and on-site construction commencing early next year. The construction of the corridors will be delivered on a phased basis in order to reduce the traffic impacts that could arise should all twelve be constructed concurrently. In relation to the Liffey Valley to City Centre Core Bus Corridor, it is likely that the Scheme will be implemented in the first half of the overall Core Bus Corridor construction programme.

During 2024, detailed communication arrangements for the construction phase will be developed to ensure that residents, community groups, businesses and public

representatives are kept fully informed and have access to relevant liaison personnel during the delivery of the Scheme.

Detailed zoomable drawings remain available on the NTA website at <https://liffeyvalleyscheme.ie/> under chapter 4.2 General Arrangement.

Q.37 COUNCILLOR VINCENT JACKSON

To ask the Chief Executive that that the loose flag stones outside the Roma / Chemist on Le Fanu Road Ballyfermot Dublin 10 which are a serious trip hazard be repaired / replaced as a matter of urgency. I was told last year these works would be done however to date they are still awaiting attention.

CHIEF EXECUTIVE'S REPLY:

For the purpose of clarification this location has been inspected twice following representations from the Councillor regarding repairs of loose concrete flags outside the above locations and on two separate occasions Road Maintenance have inspected, logged to their works list and repaired both defects.

Upon further inspection, a separate concrete flag has now worked its way loose and will be repaired in the same way. I also inspected some of the footpaths along the Ballyfermot Road where I picked up some more loose flags outside Berkeley and Larry Massey Funeral Home, which will also be added to our works list for repair.

Q.38 COUNCILLOR EIMER MCCORMACK

To ask the Chief Executive to report on when the public spaces will be power washed, the outside of the complex painted and gates fixed at Sheridan Court, Dublin 1.

CHIEF EXECUTIVE'S REPLY:

Housing Maintenance Caretaking Section will add the above complex to the works programme and it will be power hosed on rotation.

As exterior painting was carried out in 2016 this complex is not due to be painted for another couple of years.

The gates at the entrance to the car park have been repaired by Electrical Services following vandalism.

Q.39 COUNCILLOR EIMER MCCORMACK

To ask the Chief Executive to report on when the works at Botanic Avenue outside the gate of Griffith Park opposite to Mannix Road will be completed and are all works on Crawford Avenue also now fully completed?

CHIEF EXECUTIVE'S REPLY:

An Inspector from the Council's Roadworks Control Unit checked said locations on the 28th February and reported that he could not find any public domain works in progress at the time.

The Pay & Display / Permit Parking Scheme on Botanic Avenue is due to be completed by Friday the 8th of March 2024 as long as there are no cars impeding the line marking team and weather permitting. TAG will assess Crawford Avenue to confirm that all requested work is complete.

Q.40 COUNCILLOR EIMER MCCORMACK

To ask the Chief Executive to report on when will the construction commence of a sensory room and ramp at the front door (**details supplied**).

CHIEF EXECUTIVE'S REPLY:

Housing Maintenance Adaptations Section received an application for a sensory room, however this does not fall under the remit of the scheme. An extra bedroom or bathroom are provided for under the scheme. We did not receive an application/OT Report in relation to the provision of a ramp at this property. However, if the tenant wishes to provide us with same, we can have it assessed for this family.

Q.41 COUNCILLOR EIMER MCCORMACK

To ask the Chief Executive to report on what efforts can be made by Dublin City Council outside of Garda cooperation to address the issue of drug dealing and use at Blessington Street, Basin Park, The Royal Canal Bank, Western Way, and Mountjoy Street beside the TUD Grangegorman Campus and Broadstone Luas station? Is there anything can be done regarding the provision of CCTV and enhanced public lighting to discourage such antisocial behaviour.

CHIEF EXECUTIVE'S REPLY:

Under the Lighting Upgrade project the street lights on Blessington Street, Royal Canal Bank, Western Way and Mountjoy Street will be upgraded to LED street lights.

An exact timeframe for this upgrade work cannot be provided at this time except to say that this upgrade work will be carried out in the next five years or so.

In the meantime the existing street lights will continue to be maintained in accordance with existing maintenance regimes.

Q.42 COUNCILLOR DARRAGH MORIARTY

To ask the Chief Executive clear the overgrown weeds, ensure regular cleaning and remove graffiti from (**details supplied**) Residents say these laneways are regularly used by workers from the nearby NCH for after work drinking and I witnessed a number of broken glass bottles down this laneway on a recent visit.

CHIEF EXECUTIVE'S REPLY:

Waste Management Services had the above mentioned laneways cleaned on the 22nd February 2024. We will ensure that they are monitored and cleaned on a more regular basis.

I have confirmed that weekly litter picks take place along this lane from the entrance at Brookfield Street to the old Thornton's garage on SCR. Each intersection is inspected and cleaned. The broken glass observed on Thursday 29th of February has been removed. The entire lane was scraped between the end of December and the start of January and all debris removed. The graffiti is on private property and thus the responsibility of the property owner. I will speak to the contractor regarding anti-social behaviour at the laneway entrance and monitor the location going forward.

Q.43 COUNCILLOR DARRAGH MORIARY

To ask the Chief Executive to explain the rationale behind the City Council's housing allocation priority system whereby a person living long-term (6 months +) in homeless emergency accommodation (EA), who is eligible for Homeless HAP, is not deemed to be Band 1 homeless priority on the housing waiting list. Should that same person somehow find a property available for rent within Homeless HAP limits and secure a tenancy in the private rented sector with state support, they would move to Band 1 homeless priority on the housing list. Can the Chief Executive confirm if this is the case, and if so, explain the rationale? Why is somebody who has successfully moved out of Homeless EA given higher priority than somebody still stuck in Homeless EA?

CHIEF EXECUTIVE'S REPLY:

Following an amendment to the Scheme of Lettings in May 2018, there is no longer a "Homeless Priority" housing list, however, a small amount of the applicants remain on the housing list with this priority. Housing Allocations have made contact with Councillor Moriarty and have discussed this in more detail.

Q.44 COUNCILLOR DARRAGH MORIARTY

To ask the Chief Executive the following:

At the February City Council meeting, in response to a question from me during the meeting on the Social Housing Delivery Report, the Assistant Chief Executive stated that the City Council was looking at "alternative delivery streams" for the regeneration of the Dolphin House flat complex – while it is welcome in principle that alternative, presumably quicker, delivery streams are being looked at, can the Chief Executive outline what alternative streams are currently being considered?

CHIEF EXECUTIVE'S REPLY:

A non-statutory masterplan has been developed by Dublin City Council (DCC) and agreed with the local community and the Regeneration Board. This Masterplan has been reviewed and revised a number of times over the years. The most recent masterplan was agreed with the Regeneration Board in 2021.

The master plan for the regeneration of Dolphin House includes for:

- Complete regeneration of the existing estate and delivery of over 750 homes
- Both social and affordable housing to be provided
- Provision of community amenities including a community centre and park

To date, 100 new social homes have been provided in the Phase 1 regeneration project, as well as 43 Older Person homes delivered by Fold Approved Housing Body. The next step is to proceed with Phase 1B which will deliver 28 new homes. We hope to commence the Pre-Part 8 process for phase 1B in the coming months. As the project is subject to an Environmental Impact Assessment Report (EIAR) and a Part 10 planning lodgement in order for DCC to meet statutory planning requirements, no other demolition or construction can commence until this is completed.

Following a meeting with Minister O'Brien and the Dolphin House Regeneration Board in February 2023, a review was undertaken where all options were discussed and investigated with the aspiration of reducing the significant time lines involved in progressing the regeneration of Dolphin House.

The indicative timeline for the Masterplan is now as below:

Demolition of Dolphin Park to commence March 2024

Dolphin Master Plan EIAR consultant tender advertised March/April 2024

Appointment and commencement of EIAR Design Team 2025

Submission of Dolphin Master Plan Part 10 Planning application for Phases 2 & 3 mid 2026

Dolphin Phase 1b currently at pre Part 8 planning stage to deliver 28 homes in 2027

Dolphin House Phase 2 to deliver 190 homes in 2031

Dolphin Phase 3 to deliver approximately 430 affordable homes post 2031

While DCC considers every opportunity to shorten and alleviate project timescales, in construction projects of this scale and nature, we have statutory requirements to meet. These steps cannot be fast tracked and sometimes the timelines are beyond the

control of DCC. Similarly, as with all planning approvals, we may be subject to challenges that can have further impacts on delivery timelines.

In order to advance the project, the capacity of Dublin City Council's Housing and Community Services Department has to be considered. This is a major investment in housing delivery and requires a robust examination of different delivery streams available to us in order to consider how best we can deliver Phase 2 and Phase 3 so that we ensure project success in delivering quality, mixed tenure homes, within a reasonable timeframe while also achieving value for money.

With all large scale projects, delivery streams examined include via DCC Direct delivery, Developer - led, Approved Housing Bodies, Land Development Agency and Public Private Partnerships.

In order to examine this further, Public Spend Code reports are currently being progressed with our external consultants and once these have been completed and decisions are required to be made, further consultation and information will be available to the Council members and local community.

Q.45 COUNCILLOR DARRAGH MORIARY

To ask the Chief Executive to address the following **(details supplied)**:

CHIEF EXECUTIVE'S REPLY:

Road Maintenance carried out resurfacing works on the laneway between Inchicore Terrace North and Inchicore Terrace South as part of the 2022 Resurfacing contract. At present there are no plans or scope to include the resurfacing of this requested portion of laneway at **(details supplied)** in the 2024 contract. However, I will put it forward for possible inclusion in our 2025 resurfacing contract which is dependent on available funding. In the meantime, I will request the Area Inspector to visit and log any defects to our works list for a temporary repair.

Q.46 COUNCILLOR MANNIX FLYNN

To ask the Chief Executive as a matter of urgency to reinstate the bollards at ball Lane beside Glovers Court flats in order to stem the antisocial behaviour which is occurring there on a regular basis.

CHIEF EXECUTIVE'S REPLY:

The unauthorised bollards were erected on a public road by a private entity and were blocking access to a public road. They were subsequently removed by Road Maintenance Services.

Q.47 COUNCILLOR MANNIX FLYNN

To ask the Chief Executive to give an update on the motion that was agreed at the Central Area committee to take back in charge Taylors Hall under the management of Dublin City Council as a community resource centre also can give an update on the Planning Enforcement that was initiated regarding unlawful development at this historic location.

CHIEF EXECUTIVE'S REPLY:

An updated advisory warning letter has been sent to all parties that have leasehold interests in this property on Friday the 16th of February 2024 relating to the alleged unauthorised use of the lower ground floor area. As this is a live planning enforcement case no further comment can be made on the matter at this time.

Q.48 COUNCILLOR MANNIX FLYNN

To ask the Chief Executive to investigate the possibility of placing showers / wash rooms for counsellors within City Hall buildings or the Civic offices.

CHIEF EXECUTIVE’S REPLY:

Civic Offices:

We note that showering and washing facilities are available in the Civic Offices, please find attached list of toilets and showers recently circulated by facilities.

If additional sanitary facilities (or designated facilities) are required, it more feasible to retrofit these to the Civic Offices than to City Hall.

City Hall:

Part M of the Building Regulations identifies instances where achieving standards of accessibility associated with new buildings is not feasible because of particular circumstances. The following is noted in relation to protected structures; *‘Where the works would have a significant adverse effect on the historical significance of the existing building, facility or environs e.g. works to a Protected Structure’*

It is City Architects view that a fully functional, adequately accessible changing places facility or additional showers cannot be installed in City Hall without the challenges of very significant re-planning of the existing basement visitor WC facilities and the provision of new water supply and drainage into areas of the building currently without services. We are available to discuss as required.

Q.49 COUNCILLOR MANNIX FLYNN

To ask the Chief Executive Can to issue a report as to how many staff at Dublin City Council have reported attacks or threats upon them in the past two years also to include this report how many times Dublin City Council staff had to call on Garda Siochana to an assist them in the exercise of their duty or in order to protect them from assault attack,or treat.

CHIEF EXECUTIVE’S REPLY:

In 2022 there were 91 cases of reported attacks / threats upon DCC Staff. In 2023 there were 140.

We (Health, Safety & Wellbeing Unit) do not gather information relating to calls made to An Garda Siochanna so would not be in a position to comment on same

Q.50 COUNCILLOR SÉAMAS MCGRATTAN

To ask the Chief Executive to consider buying the **(details supplied)** for the provision of senior citizen accommodation in the area.

CHIEF EXECUTIVE’S REPLY:

We note the Councillors request, and we will investigate this proposal.

Q.51 COUNCILLOR SÉAMAS MCGRATTAN

To ask the Chief Executive what position is **(details supplied)** on the financial contribution scheme and if he could be considered for one of the new units in St Finbarrs Court.

CHIEF EXECUTIVE’S REPLY:

The above applicant is on the Financial Contribution housing list, with an application date of 22/03/2016, the applicant holds the following position on this list;

Area	Bedsizes	Position
E	1	59

The applicant’s interest in the new units in St Finbarr’s Court has been noted on his file.This development is scheduled to be complete December 2024 for 46 older person

units. Dublin City Council allocates properties based on time on the list and currently there are applicants on the waiting list of longer standing who have to be considered as suitable vacancies arise. It is not possible to indicate accurately the prospect of an offer for any applicant. The number of vacancies arising will be a determining factor in the length of time applicants could expect to be on the waiting list.

Q.52 COUNCILLOR SÉAMAS MCGRATTAN

To ask the Chief Executive for an update on when **(details supplied)** will be reached as part of the ongoing insulation programme.

CHIEF EXECUTIVE'S REPLY:

Housing Maintenance Energy Liaison Officer will make contact with the tenant with a view to carrying out a site survey on the property in Q2/Q3 2024. Should the property be deemed eligible for inclusion in our Energy Efficiency Retrofitting Programme, we will endeavour to include the property in our 2024/2025 programme of works.

Q.53 COUNCILLOR SÉAMAS MCGRATTAN

To ask the Chief Executive could Dublin City Council develop the waste land between **(details supplied)** into a community facility for Dunard.

CHIEF EXECUTIVE'S REPLY:

Dublin City Council does not own the land in question. Should the land become available to us we would give consideration to the suitability of the site, the type of resource required and the availability of the finance to construct any such facility.

Q.54 COUNCILLOR NIAL RING

To ask the Chief Executive in accordance with Section 15 of the Local Government Rates and Other Matters Act 2019, can now prepare a scheme providing for the waiver of a portion of Commercial Rates due to it from businesses along the Clontarf to City Centre Cycle Route , and if necessary, write to the Minister for Housing, Local Government and Heritage to make any necessary regulations, as provided for under s.15 (5) of the Local Government Rates and Other Matters Act 2019, to enable Dublin City Council to prepare and implement such a scheme.

The construction of the €63m cycle route, together with ancillary works, along Fairview, North Strand and Amiens Street has resulted in a dramatic downturn in business for the retailers etc. along the route (some have ceased trading). A recent survey undertaken by me shows an average decrease in turnover of over 30%, with some businesses recording over 75% decreases.

Finally, does the Chief Executive agree that it would be reasonable for Dublin City Council to waiver an agreed amount of rates to compensate these businesses for the fall off in trade and profits over the past 18 months, caused directly by the construction of the cycle route.

CHIEF EXECUTIVE'S REPLY:

There have been previous calls for compensation payments for businesses impacted by works related to the C2CC project. It is understood that this matter has been the subject of discussion at meetings of the North Central Area committee. There has been reference to both specific works at the Bus Gate at Main Street Belmayne and also to the C2CC project works in Fairview.

The City Council recognises that infrastructural projects may cause disruptions to both businesses and citizens and can be difficult to live with but such projects nonetheless are essential to the future economic life and development of the city. During the course of major infrastructure projects and other civic improvement works it is almost inevitable that some ratepayers will be subjected to inconvenience despite the best efforts to keep such inconvenience to a minimum. In a city of Dublin's size many ratepayers will at some time be affected by such projects or works. However the long-term benefits to businesses arising from these works greatly outweigh the inconvenience caused during their construction phase.

DCC project staff, whether in respect of the C2CC project team, the Belmayne Bus Gate project or any other, make every best effort to complete the works at the earliest opportunity so that benefits can accrue to businesses, residents and visitors. It is worthwhile to clarify the legislative framework relating to commercial rates. Rates are a tax on property. The City Council at the Statutory Budget meeting determines the rate on valuation each year which is applied to the valuation of all rateable properties and from which commercial rates are determined. There is no legislative provision or discretion to waive or reduce rates on any business in regard to any mitigating circumstances and therefore rates liabilities cannot be reduced. In situations where financial pressures are impacting on the business flexibility, however limited, is offered through engagement between the ratepayer and the rate collector resulting in a mutually agreeable arrangement to discharge the outstanding rates over a period of time. All ratepayers are urged to engage with their rate collector.

There are no exchequer financial supports to businesses in such circumstances. It must be noted that were a mechanism to be provided by Central Government to reflect disruption to business during project construction phase, it would be appropriate that such a scheme would also reflect the benefits to business (betterment) post project completion.

Both schemes referenced are funded by the National Transport Authority (NTA). I have not been advised of proposals being considered by either the Minister for Transport or the NTA to introduce a national a scheme of compensation related to the negative impacts on local businesses during implementation of transport related works.

The Local Government Rates and Other Matters Act 2019 was passed by the Oireachtas and enacted on 11th July 2019. The purpose of the Act is to modernise the collection of rates, which are a vital source of funding for local authorities representing approximately 30% of the revenue income across all local authorities. The Act contains a provision in section 15 relating to potential alleviation schemes. These provisions empower the Elected Members of local authorities to devise and achieve policy objectives through locally targeted rates waiver schemes. This will enhance the capacity of local authorities to address important issues and challenges particular to their local areas and to enable them to support specific objectives to promote community, social and economic development, urban planning.

The regulations to be made under this section will specify the maximum percentage of waiver and other matters particular to the operation of alleviation schemes, which may include time period applying to schemes, the process and sequencing to be followed, public consultation and the review and report on completion of schemes. The elected members will have discretion to introduce waiver schemes that support objectives outlined in the City Development plan, local area plans, the Local Economic and Community plan or the National Planning framework. Section 15 was commenced on 1st January 2024. Dublin City Council understands that the intention of the Minister

and Department of Housing is that **section 15 will apply from 2025 for rates waiver schemes**. The making of such a scheme must be closely aligned to the preparation of the draft budget for the financial year. Section 15 of the Act is set out below for information purposes.

Section 15:

Subject to subsection (4) and in accordance with such regulations as may be made under this section, a local authority may make a scheme providing for the waiver by the authority of all or a portion of the rates due to it by ratepayers in order to support the following:

- a. the implementation of the National Spatial Strategy within the meaning of the Act of 2000
- b. the implementing of a development plan within the meaning of the Act of 2000
- c. the implementation of a local area plan within the meaning of the Act of 2000
- d. the implementation of a local economic and community plan within the meaning of the Act of 2001.

The making of a scheme under this section shall be a reserved function within the meaning of the Act of 2001.

A local authority may amend a scheme under this section.

A local authority shall not make a scheme under this section until the adjustment period within the meaning of section 20 of the Act of 2014 ceases for every specified area within its administration area of the authority.

The Minister may make regulations for the purposes of this section and in particular but without prejudice to the foregoing regulations under this section may make provision in relation to all or any of the following matters:

- a. the specification of the maximum percentage of rates that may be waived by a local authority under this section
- b. the period in relation to which a scheme may be made and
- c. any other matters which appear to the Minister to be necessary or expedient for the purposes of this section.

Q.55 COUNCILLOR NIAL RING

To ask the Chief Executive in relation to the Dublin City Centre Transport Plan can the Chief Executive answer the following regarding the consultation process and the online survey?

- a. Does the Chief Executive agree that the consultation process was inherently discriminatory and non-inclusive given that the consultation page was online only, thus excluding the significant proportion of the population who have no access to or ability to use the internet (I refer to the Age Action report "Digital Inclusion and an Ageing population which concluded that 65% of people over 65 experience digital exclusion and that 25% of people aged 60-74 and 56% of people aged 75+ are not using the internet)
- b. Can the Chief Executive explain why the entire consultation promotion consisted of an information desk in the atrium of Dublin City Council's Civic Offices, social media posts, two media releases and 6 "events", such "events" attracting c.451 attendees, of which 100 were Dublin City Council staff?
- c. I note the report mentions that the draft plan was covered by RTE and "it reached 1.4m people". This "reach" figure must be put in context with the fact that only one

quarter of one percent of these "reached" participated in the consultation process. Could the Chief Executive comment?

c. Does the Chief Executive agree that this lack of promotion was designed to ensure that minimal information about the plan would be made public, thus ensuring that a broad and representative opinion on the plan would be received?

d. Can the Chief Executive explain why no information events were held in public libraries, colleges and local community centres as was the case, for example, with the Climate Action Plan consultation process?

e. In relation to the online survey, does the Chief Executive agree that this, just like the overall consultation process, was both discriminatory and non-inclusive (as (a) above)? The fact that it was an online survey surely answers this question but it would be useful to get the Chief Executive's response.

f. Who designed the survey in terms of layout, questions and reply mechanism?

g. How much did the survey design etc. cost?

h. Does the Chief Executive agree that the first six questions were all prompted, closed and heavily loaded?

i. Does the Chief Executive agree that the way these questions were asked resulted in fundamental biased answers given that they were prompted and loaded?

j. In light of the ongoing controversy about the draft plan and the very real facts emerging that the process was fundamentally flawed, would the Chief Executive agree that the consultation process should be undertaken again to ensure that all opinions are sought, that a non-biased survey is conducted so that, inter alia, the public concerns that the process was covert and clandestine would be alleviated. The very reputation of Dublin City Council and the way it engages with the people of Dublin is at stake here.

CHIEF EXECUTIVE'S REPLY:

The consultation documentation material was available for the public to view, read, download and make submissions through a range of options including online, email, post and in person at information events and stakeholder meetings. This consultation process incorporated all the normal and standard elements DCC avail of to communicate to the public. All the information and documentation was available both in the Civic Offices and the offices of the NTA as well as online. As part of the process people did attend and make written comments having studied the documentation available to them. They also could send us a response by post and people chose to also do this.

The consultation process was put in place following the Transportation SPC meeting and there were press releases, which included details of how the public could view the documentation and give their feedback. There was wide spread media coverage of the draft city centre transport plan. The public could view the plans at the DCC and NTA offices. All Councillors were informed of the consultation process and were updated in the monthly report and, also the quarterly traffic report to the full City Council meeting, where Councillors had the opportunity to raise any issues regarding the City Centre plan. No observations or concerns were received from Councillors at any stage during the process.

The figures for the reach of the consultation is based on our press office figures. The participation figures for this consultation were very high in comparison, for example to the Climate Action Plan consultation, which received 88 submissions in comparison to the City Centre Transport Plan, which received over 3,500 observations. It should also be noted that one submission may represent multiple organisations for example the Health Alliance submission represented multiple health organisations.

Consultation sessions were held with any organisation that requested consultation, for example the Westbury, and briefing sessions were also held with all the main business organisations, i.e., IBEC, Chamber of Commerce, Dublin Town, Lord Mayor's Forum and An Garda Síochána.

The consultation process was fair open and representative, and it was not exclusively an on-line survey as people could fill in the questionnaire in person, or post in their submissions.

The layout was designed in house.

As above.

The questions were based on the text of the draft Dublin City Centre Transport Plan and the purpose and objective of the Plan was set out clearly in the Transport Plan as an objective of the Dublin City Development Plan.

There was no requirement to fill out the questionnaire and people who disagreed with the plan had multiple ways of expressing their preference, in person, by post, by email and on line.

We do not agree that the consultation process was flawed and the fact that the Draft City Centre Transport Plan was out for consultation for 11 weeks, which is considerably longer than an average consultation period, was referenced by all the main media in Ireland and in the end attracted 40 times more submissions than the Climate Action Plan, which was also out to consultation around the same time, shows that the process worked extremely well. A lot of citizens took the time and trouble to make their views known to us both for and against and it would be completely wrong in our view to re-open the consultation when so many people have already taken part.

It should be noted that the various different measures contained in the plan will go through the appropriate statutory route, so for example the final College Green Dame Street plaza design will have to go through the necessary and appropriate public consultation, and therefore, will provide another opportunity for the public to express their views and to have their say.

Q.56 COUNCILLOR DEBORAH BYRNE

To ask the Chief Executive in light of the rising number of lethal and sometimes fatal active traffic collisions, whether Dublin City Council in partnership with a Gardai Síochána would engage with the team behind the newly launched Collision Tracker in Dublin City to allow citizens to report active travel collisions, hazards and near misses. This could help to identify a greater number of collision hotspots, improve infrastructure, penalise offenders and promote safer, greener travel in Dublin City.

CHIEF EXECUTIVE'S REPLY:

Following a road fatality, the DCC road safety engineer, in collaboration with An Garda Síochána, carry out a site investigation and complete the LA 16 form. The outcome of the site investigation is given to the Road Management Office. The road safety section is waiting for the final data collision from the Road Safety Authority.

Currently, the road safety engineer is conducting a report based on analyzing the Regional Road Network Safety Analysis report for Dublin City Council from 2017-2019. The analysis will focus on the top 10 locations with the highest incidence of accidents. Through this analysis, the Environment and Transportation Department aims to determine where immediate actions should be taken to begin a program of interventions where a specific risk has been identified.

Q.57 COUNCILLOR DEBORAH BYRNE

To ask the Chief Executive regarding D-Light studios if, in light of the shortage of artist spaces in the city, D-Light's monumental efforts and significant expense spent refurbishing a once-derelict building and providing valued services at a sliding scale to the local community, and in recognition of the fact that its closure would be hugely detrimental to the local area, whether Dublin City Council will make every effort to ensure that D-Light Studios can remain in-situ, undertaking the required fire safety works as a matter of urgency while respecting their occupancy, and providing them with a long-term lease.

CHIEF EXECUTIVE'S REPLY:

Darby Mullen, Senior Executive Engineer, Climate Action Office, Dublin City Council attended the recent UTRAP meeting on 20/10/2023. Dublin City Council updates and progress reports implementing the measures outlined in Annex 1 of the report were provided to the group.

The DCC actions required include the following;

Recommendation 2- The Air Quality Monitoring and Noise Control Unit and the EPA are carrying out a targeted indicative monitoring programme (Diffusion Tube Survey) in the Dublin 7 area for 2024.

Recommendation 9 - The Air Quality Monitoring and Noise Control Unit and the EPA held a meeting on 12/02/2024 to discuss air quality modelling. DCC will provide the data needed for this including traffic counts.

Recommendation 10- On the 17th February 2020 the four Local Authorities that make up the Dublin region signed a commitment to the Breathe Life Campaign and to meet World Health Organisation (WHO) air quality guideline values by 2030.

More information about the WHO levels can be viewed at

<https://www.who.int/news-room/feature-stories/detail/what-are-the-who-air-quality-guidelines>

In 2019 there was a breach of the Air Quality Standards limit value for NO₂ at the Saint Johns Road Air Quality Station. The NO₂ limit value averaged over a calendar year is 40 micrograms per metre cubed. The level recorded in 2019 was 43 ug/m³ (micrograms per metre cubed)

This necessitated writing an Air Quality Management Plan and annual progress reports are provided to the EPA.

The air quality has improved on Saint Johns Road since the plan was written and implemented and in 2023 a mean NO₂ level of 32 ug/m³ was recorded at the Saint Johns Road Site.

This is an improvement of 25 %

This improvement is evidence that implementing the measures in the Plan is reducing air pollution in the City.

Dublin City Council is committed to continually implement these measures to achieve further improvement and is committed to the measures outlined in the following existing plans, projects and strategies

Dublin City Centre Transport Plan 2023
The Active Travel Programme

The Greater Dublin Area Transport Strategy 2022 – 2042
The Dublin City Council Climate Action Plan 2024 – 2029

Q.58 COUNCILLOR DEBORAH BYRNE

To ask the Chief Executive what action(s) have been taken by Dublin City Council on foot of the recommendations of the 2023 UTRAP synthesis report and to ask for evidence of the improvement in air quality in parts of the city on foot of this action and if he will comment on the matter.

CHIEF EXECUTIVE'S REPLY:

Darby Mullen, Senior Executive Engineer, Climate Action Office, Dublin City Council attended the recent UTRAP meeting on 20/10/2023. Dublin City Council updates and progress reports implementing the measures outlined in Annex 1 of the report were provided to the group.

The DCC actions required include the following;

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The Greater Dublin Area Transport Strategy 2022 – 2042
The Dublin City Council Climate Action Plan 2024 – 2029

Q.59 COUNCILLOR DEBORAH BYRNE

To ask the Chief Executive if actions have been taken, specifically for St Johns road west, to reduce nitrogen oxide concentrations, given that the UTRAP group was originally formed on foot of the air quality breaches found on that road in 2019 and to ask for the evidence of improvement in air quality on that road on foot of these actions and if he will comment on the matter.

CHIEF EXECUTIVE'S REPLY:

In 2019 there was a breach of the Air Quality Standards limit value for NO₂ at the Saint Johns Road Air Quality Station. The NO₂ limit value averaged over a calendar year is 40 micrograms per metre cubed. The level recorded in 2019 was 43 ug/m³ (micrograms per metre cubed)

This necessitated writing an Air Quality Management Plan and annual progress reports are provided to the EPA.

The air quality has improved on Saint Johns Road since the plan was written and implemented and in 2023 a mean NO₂ level of 32 ug/m³ was recorded at the Saint Johns Road Site.

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The Active Travel Programme
The Greater Dublin Area Transport Strategy 2022 – 2042
The Dublin City Council Climate Action Plan 2024 – 2029

Q.60 COUNCILLOR DANIEL CÉITINN

To ask the Chief Executive if Stannaway Avenue could be included in the 2024 Footpath Resurfacing Programme

CHIEF EXECUTIVE'S REPLY:

The Area Engineer will carry out an inspection of Stannaway Avenue for potential inclusion in the 2025 programme.

Q.61 COUNCILLOR DANIEL CÉITINN

To ask the Chief Executive if the bollards at the Derravaragh Road/Aideen Drive & Mount Tallant Avenue/Derravaragh Road junctions could be repainted, and if planters could also be installed at the bollards in conjunction with this.

CHIEF EXECUTIVE'S REPLY:

Will have this checked within 30 days of today's date 29/02/24.

Q.62 COUNCILLOR DANIEL CÉITINN

To ask the Chief Executive if the footpaths on the corners of Ashfield Park at the junction with Ashdale Road could be resurfaced as they are very badly damaged.

CHIEF EXECUTIVE'S REPLY:

The Area Engineer has recommended that pavements either side of Ashfield Park at the junction with Ashdale Road be renewed. The footpaths have been added to the works list and will be completed as soon as possible

Q.63 COUNCILLOR DANIEL CÉITINN

To ask the Chief Executive can he arrange for the stairwells in (**details supplied**) to be power washed and to be put on a regular cleaning schedule.

CHIEF EXECUTIVE'S REPLY:

Housing Maintenance Caretaking Section are in the process of completing a deep clean of this complex including weeding, scutching (removal of moss from brickwork) and power-washing of all shared areas.

Q.64 COUNCILLOR JAMES GEOGHEGAN

To ask the Chief Executive if Oaklands Crescent, Oaklands Drive and Oak Apple Green, Rathgar, Dublin 6 can receive a clean sweep and the roads be cleaned.

CHIEF EXECUTIVE'S REPLY:

Waste Management Services had Oaklands Crescent and Oakslands Drive swept and cleaned on the 22nd February 2024. Oak Apple Green is a private estate which doesn't come under our cleaning remit.

Q.65 COUNCILLOR JAMES GEOGHEGAN

To ask the Chief Executive if he can outline when Road Maintenance intend installing a road side gully in the location provided in the photo attached which is on Lower Baggot Street and opposite Doheny and Nesbitts pub.

CHIEF EXECUTIVE'S REPLY:

Road Maintenance Services will carry out a level survey and subject to the findings consider the installation of a gully in conjunction with our drainage colleagues.

Q.66 COUNCILLOR JAMES GEOGHEGAN

To ask the Chief Executive if he can set out in tabular form all new pedestrian crossings that have been installed in Dublin 4 and Dublin 6 between June 2019 and today's date.

CHIEF EXECUTIVE'S REPLY:

Please find below in tabular form all new pedestrian crossings that have been installed in Dublin 4 and Dublin 6 between June 2019 and today's date.

Location.	Area.	Works Description.
Sandymount Green @ 5 Sandymount Green (Old Borzas Burger).	D4	Controlled Pedestrian Crossing.
Sandymount Green @ Bennetts Auctioneers.	D4	Controlled Pedestrian Crossing.
Palmerston Road / Cowper Road.	D6	Controlled Pedestrian Crossing.

Thorncastle St. / Bridge St.	D4	Controlled Pedestrian Crossing.
Mountpleasant Avenue / Richmond Hill.	D6	2 No. Uncontrolled Pedestrian Crossings.
Mount Drummond Square.	D6	Uncontrolled Pedestrian Crossing.
Seafort Avenue.	D4	Zebra Crossing.

From June 2019 to date there have being 4 No. Controlled Pedestrian Crossings, 3 No. Uncontrolled Pedestrian Crossings and 1 No. Zebra Crossing installed in the Dublin 4 / Dublin 6 Area.

Q.67 COUNCILLOR JAMES GEOGHEGAN

To ask the Chief Executive if he can set out in tabular form all roads that have been resurfaced (full resurface not pothole filing) and all footpaths that have been resurfaced (full repair programme) in Dublin 4 and Dublin 6 between June 2019 and today's date.

CHIEF EXECUTIVE'S REPLY:

The Area Engineer has emailed Cllr. Geoghegan the information requested on 26/2/2024.

Q.68 COUNCILLOR CAT O'DRISCOLL

To ask the Chief Executive to provide a report on the Dublin Cycling Buddy App to include current users, use of the feedback provided by users and future plans for the project.

CHIEF EXECUTIVE'S REPLY:

A reply to this question will be issued to the Councillor by the 14th Of March.

Q.69 COUNCILLOR CAT O'DRISCOLL

To ask the Chief Executive to progress the taking in charge of Fairfield Court, Fairfield Road, Glasnevin, Dublin 9, to progress the switching of public lighting and installation of yellow lines at the entrance and to provide a report on the process to date.

CHIEF EXECUTIVE'S REPLY:

The Planning department have no records of a Taking in Charge application being submitted from the developer or the homeowners for the services in Fairfield Court to be taken in charge. I have attached the Taking in Charge Procedure if the homeowners want to initiate the process for the services to be taken in Charge.

Q.70 COUNCILLOR NIAL RING

To ask the Chief Executive if, following on from the reply to my question No.47 last month, if Dublin City Council could give their tenants a vote as Proxy as the tenant would be in the best position to vote at AGSs etc.

CHIEF EXECUTIVE'S REPLY:

As previously noted Dublin City Council owns properties in a large number of multi-unit schemes across the City and it may not always be practical for the Council to participate in an AGM. However if the tenant wishes to advise Housing Maintenance on their views this will be considered and, if practical to arrange, Dublin City Council,

as the owner of the property concerned will reflect on this and vote accordingly at the AGM.

Q.71 COUNCILLOR NIAL RING

To ask the Chief Executive if there are any Dublin City Council employees seconded to other local authorities, state agencies, NGOs or government departments and likewise if there are employees of similar organisations seconded to Dublin City Council. Also, to ask the Chief Executive if any employees working for Dublin City Council are being paid, either directly or indirectly, by any such bodies.

CHIEF EXECUTIVE'S REPLY:

There are a number of secondment arrangements in place in Dublin City Council. Dublin City Council currently has 6 employees employed on a secondment basis from other public bodies, and 7 employees seconded to other public bodies. Secondments are agreed for a specific period of time and are a blend of Professional/Managerial/Administrative staff. The salaries associated with each secondment are paid by the permanent employer and the salary recouped from the secondment organisation.

Q.72 COUNCILLOR NAOISE Ó MUIRÍ

To ask the Chief Executive please respond on tree-related issues on Calderwood Road and Griffith Avenue as set out in the correspondence below (**details supplied**)

CHIEF EXECUTIVE'S REPLY:

The 2 small purple plums were removed as they were picked up in the tree survey of the area as having significant decay and structural issues. The condition of these 2 specimens was noted during a joint inspection with the residents association last year with significant decline, cavities and decay clearly evident.

The challenges in planting reported gaps in the tree planting along the boulevard was discussed on site last year with the residents association. The sheer size and shade cast (and water competition) due to the existing trees renders many of the verges identified as planting opportunities as unsuitable for new avenue tree planting. Newly planted plane trees will not establish or thrive in the deep shade cast by the existing mature trees here as has been seen with the excessively high failure rates of the trees that were planted in empty verges over the last 2 years.

There are no proposals for tree planting here in the coming weeks as the priority is scheduling the necessary pruning works identified in the tree survey. Approximately 200 crown reductions along the Avenue have been included in a tender just published to continue this work. The area will be inspected later in the summer and any viable tree planting opportunities listed for inclusion in next winter's tree planting programme.

07WT	SIDE OF	123	CALDERWOOD GROVE	North central	Prunus cerasifera 'Pissardii'	Crown and stem failure
07X0	FRONT OF	16	CALDERWOOD ROAD	North central	Prunus serrulata Kanzan	Nylon sounding hammer indicates significant stem decay, hollowing, full crown with dense foliage, Laetiporus sulphuris on old pruning wounds on stem.
07X1	FRONT OF	15	CALDERWOOD ROAD	North central	Prunus cerasifera 'Pissardii'	Crown and stem failure, cavity with decay at unions
07X6	FRONT OF	27	CALDERWOOD ROAD	North central	Prunus cerasifera 'Pissardii'	Crown and stem failure with cavity/decay at unions

Q.73 COUNCILLOR NAOISE Ó MUIRÍ

To ask the Chief Executive confirm the status of the following vehicle (**details supplied**) and if abandoned organise for its removal from Belgrove Road.

CHIEF EXECUTIVE'S REPLY:

This is already under investigation since 20.2.2024 through Citizenhub request.

The following is the process:

Approximately 2-3 days from first report on Citizenhub I correspond with the registered owner through details sourced from Motor Tax Office post first inspection.

I then allow 14 days for reply before revisiting the vehicle where if still in situ I affix a 7 day removal notice. In real time this can lead to a fortnight or more to identify if the vehicle is actually abandoned depending on contact from original correspondence and interested parties who see my notice and feel the need to share their information, however insightful it may be.

The vehicle mentioned below is due reassessment / notice in early March (reported 16.2, inspected 20.2 and written out to 21.2). This investigation is relatively straightforward in that the vehicle is secure (locked) and has no broken glass creating health and safety issues.

Q.74 COUNCILLOR NAOISE Ó MUIRÍ

To ask the Chief Executive to comment on local residents' concerns that discarded nitrous oxide (laughing gas) canisters are being dumped in All Saints Park? Are Dublin City Council Parks aware of this issue and can Dublin City Council officials advise local residents as to how best deal with the material appropriately.

CHIEF EXECUTIVE'S REPLY:

The Parks Service is aware of this behaviour which unfortunately is becoming more and more common across the area. Advices with regard to best practice for the disposal of these and indeed any other pressurised container is that they be brought to a recycling centre as if they are disposed of in a domestic bin or skip there is a risk of explosion if they are crushed.

Q.75 COUNCILLOR NAOISE Ó MUIRÍ

To ask the Chief Executive organise for the tree outside No 4 Marino Park Avenue to be pruned back as it is over-bearing and causing loss of natural light at this location.

CHIEF EXECUTIVE'S REPLY:

While pruning works are not normally carried out to address shading of direct sunlight an inspection will be arranged in the coming weeks and any works deemed necessary will be listed for inclusion in the prioritised tree care programme for the area 2024 – 25.

Q.76 COUNCILLOR JANET HORNER

To ask the Chief Executive For an update on the status of the following properties (details supplied 1, 2&3)

Details supplied 1: 19 & 21 Connaught Street

Details supplied 2: 8 & 10 Ferguson Road

Details supplied 3: 414 North Circular Road

CHIEF EXECUTIVE'S REPLY:

Details supplied 1 is complete and the property is currently being lived in.

Details supplied 2 have a design, however when HM were due to start on site, there was an issue with the foundations and front façade of the buildings, so we are currently implementing emergency works to safe guard the structure of the buildings.

Details supplied 3, these 3 properties came as a package. No. 12 is currently being refurbished, however similar to details supplied 2, the structure of Nos. 8 and 10 are currently being reviewed.

Q.77 COUNCILLOR JANET HORNER

To ask the Chief Executive how Dublin City Council tracks and enforces compliance with the parking obligation outlined in Section 7(c) Control of Stationless On-Street Bicycle Hire Bye-Laws 2017.

CHIEF EXECUTIVE'S REPLY:

DCC has a Service Level Agreement with both licensed stationless bike hire operators, Bleeper and Moby. Under this SLA operators have a certain amount of time to remove/relocate badly parked bicycles (all bikes have to be tethered to a Sheffield stand). If operators do not remove/relocate a bicycle within the set timeframe DCC has the power (under Section 71 of the 1993 Roads Act) to remove the bicycle and charge a fee for removal and storage. In addition both Moby and Bleeper have a fines system in place for users who do not park bikes properly with recurring bad parking resulting in a user being barred from the service. Bleeper, Moby and DCC work in partnership with each other and since the service commenced in 2017 there have been very few instances of bad parking due to the DCC bye-laws stipulating that bikes must be tethered to a Sheffield stand. DCC was the first local authority in Europe to adopt this position which is now being adopted across Europe.

Q.78 COUNCILLOR JANET HORNER

To ask the Chief Executive how many licences for slot machines and gaming halls have been issued in the Dublin City Council area and in Dublin 1 specifically? And how many enforcement actions have been taken against unlicensed gaming halls in the past year?

CHIEF EXECUTIVE'S REPLY:

Part III of the Gaming and Lotteries Act, 1956 concerns the licensing of amusement halls and funfairs. There is currently no resolution by Dublin City Council in force adopting Part III of this Act. As a result there are a number of applications which have been refused by the District Court and which are currently under appeal. The appeals have yet to be heard.

The Local Authority is a notice party to applications for gaming licences under 16 of the 1956 Act, but it would have no other role in the issuing of the gaming licences. This would be a matter for the Revenue Commissioners. The Revenue Commissioners are obliged since 2019 to have a Register of Gaming Licence applications. I note that there are no details of any Gaming Licences for Dublin on the Register. <https://www.revenue.ie/en/corporate/information-about-revenue/statistics/excise/licences/gaming-licences.aspx>.

The grant of a new Gaming Licences is a matter for the District Court on notice to DCC. No new licences have been granted in DCC area in the past number of years due to the fact that there is no resolution of the elected members adopting Part 3 of the Act of 1956. DCC has no role in enforcement for unlicensed premises other than under the planning code if there is an unauthorised use. Planning Enforcement may have a role if there is an unauthorised use of a particular premises. Planning Enforcement have confirmed that they have no current enforcement files relating to unauthorised use of premises for gaming purposes.

Q.79 COUNCILLOR KEITH CONNOLLY

To ask the Chief Executive his plans to implement illegal dumping strategies and the next steps following the passing of the LGMA report

CHIEF EXECUTIVE'S REPLY:

With regard to the Codes of Practice for the operation of CCTV;

1. The WERLA office with the support of the LGMA are working to develop a suite of documents to assist Local Authorities in the implementation of the Codes of Practice for the operation of CCTV. These documents will include, inter alia, templates for Data Protection Impact Assessment (DPIA), CCTV Scheme, Data processing agreement.
2. An Oversight Board will be established which will include senior management such as the Data Processing Officer, The Head of Information Services and Executive Managers.
3. Business units will forward a business case for a CCTV scheme for approval to the Executive Manager in whose area the CCTV scheme is planned.
4. The business case will then be forwarded to the oversight Board for their approval.
5. Business case then forwarded to the Chief Executive for his approval.

Q.80 COUNCILLOR KEITH CONNOLLY

To ask the Chief Executive to provide a breakdown of the number of dog fouling and illegal dumping fines per LEA for the past 5 years.

CHIEF EXECUTIVE'S REPLY:

Number of fines issued under Section 22 (Dog Fouling) of the Litter Pollutions Acts 1997 – 2009 over the last 5 years are as follows:-

	Central	N.Central	N.West	S.Central	S.East	Total
2019	0	0	0	0	0	<u>0</u>
2020	1	0	0	1	0	<u>2</u>
2021	0	0	0	0	0	<u>0</u>
2022	0	0	0	0	0	<u>0</u>
2023	0	4	2	0	0	<u>6</u>

Number of fines issued under Section 3 (Illegal Dumping) of the Litter Pollutions Acts 1997-2009 are as follows

	Central	N.Central	N.West	S.Central	S.East	Total
2019	151	35	31	86	119	422
2020	95	151	204	127	152	729
2021	193	147	118	331	118	907
2022	202	180	19	237	165	803
2023	312	92	6	345	354	1109

Q.81 COUNCILLOR KEITH CONNOLLY

To ask the Chief Executive to provide a breakdown of the number of claims against Dublin City Council in relation to falls and trips on pathways and carriageways for past 5 years.

CHIEF EXECUTIVE'S REPLY:

Outlined below are the number of claims against Dublin City Council in relation to falls and trips on pathways and carriageways for the past 5 years.

	YEAR	NUMBER OF CLAIMS
	2019	572

	2020	455
	2021	447
	2022	413
	2023	350

Claim received numbers do not reflect payment made in a given year.

Q.82 COUNCILLOR BRIEGE MACOSCAR

To ask the Chief Executive to investigate whether lighting improvements via lighting upgrades and canopy pruning can take place on (**details supplied**) as the street is very dark due to the dense tree canopies which cover much of the lights and often stretch across the entirety of the carriageway. The area has experienced a spate of break-ins and attempted break-ins.

CHIEF EXECUTIVE'S REPLY:

Parks services will arrange for an examination of the trees along (**details supplied**), any necessary works will be listed in our Tree Care programme for the area. All trees works are carried out on a prioritised basis subject to available budget.

Q.83 COUNCILLOR BRIEGE MACOSCAR

To ask the Chief Executive advise if work can take place on the trees on (**details supplied**) particularly across from number 1 as it is extremely large for an estate of this nature.

CHIEF EXECUTIVE'S REPLY:

Parks services carried out a survey of (**details supplied**) and a number of trees along (**details supplied**) including the subject tree above have been listed for pruning in our programme of works for the year.

Q.84 COUNCILLOR BRIEGE MACOSCAR

To ask the Chief Executive to advise if interventions are possible to deal with illegal parking on (**details supplied**) as there is frequent double parking during school collection and drop off times making access for residents difficult and causing concerns about access for emergency vehicles

CHIEF EXECUTIVE'S REPLY:

Parking Enforcement Part Reply:

We will arrange for the Parking Enforcement Inspector to monitor the parking situation on (**details supplied**) during school drop off and collection times and to arrange the necessary enforcement action.

School Zone Part Reply:

As part of the Safe Routes to School programme, funded by the National Transport Authority, DCC implements School Zones which aim to improve safety for students at the school gate. For 2024 DCC has been allocated funding to install School Zones at 12 schools in Dublin city. As schools on (**details supplied**) are not on the list for this year, the team is not in a position to install a School Zone at this location at the moment.

Q.85 COUNCILLOR BRIEGE MACOSCAR

To ask the Chief Executive to advise if the right turn filter light at the junction of (**details supplied**) can always be used as there is often dangerous situations where drivers turn right due to a green light for traffic proceeding south even though the pedestrian light for people crossing St Pappin's Road is also green.

CHIEF EXECUTIVE'S REPLY:

The right-turn filter for traffic turning right from (**details supplied**) runs when demanded by traffic activating the detector loop in the right turning lane. Operating the right-turn filter every phase whether it was demanded or not would result in delays to pedestrians, public transport and all other users of the junction. Motorists who turn right when only a straight ahead green arrow is lighting are clearly breaking the law which is a matter for enforcement by An Garda Síochána.

Q.86 COUNCILLOR ANTHONY CONNAGHAN

Chun iarraidh ar an bPríomhfheidhmeannach socrú a dhéanamh (sonraí curtha ar fail) go dtógfar an crann os coinne 22 Bóthar Dhún Sinche anuas (Sa Pairc Farnham). Gealladh é seo roimhe seo mar go bhfuil sé contúirteach

To ask the Chief Executive to arrange for the tree opposite to (**details provided**) to be taken down. This was promised before because it is dangerous.

CHIEF EXECUTIVE'S REPLY:

De réir ár dtaifid, bhain Parks géag chontúirteach de chrann os coinne bóthair (sonraí tugtha) anuraidh. Déanfar iniúchadh arís ar lócas na bpáirceanna agus déanfar aon oibreacha crann is gá a sceidealú.

According to our records, Parks removed a dangerous limb from a tree opposite (**details supplied**) road last year. Parks will have the locus inspected again and any necessary trees works will be scheduled.

Q.87 COUNCILLOR ANTHONY CONNAGHAN

Iarraidh ar an bPríomhfheidhmeannach socrú a dhéanamh chun na crainn a ghearradh ag (sonraí tugtha)

To ask the Chief Executive to arrange for the trees to be cut at (**details supplied**)

CHIEF EXECUTIVE'S REPLY:

Déanfar cigireacht ar na crainn ar an bhfaiche thall (sonraí tugtha) sna seachtainí atá romhainn de réir mar a cheadaíonn an sceideal, áireofar aon oibreacha riachtanacha inár gclár oibreacha crann tosaíochta don cheantar.

The trees on the green opposite (**details supplied**) will be inspected in the coming weeks as schedule allows, any necessary works will be included in our prioritised tree works programme for the area.

Q.88 COUNCILLOR ANTHONY CONNAGHAN

Iarraidh ar an bPríomhfheidhmeannach balla an ghairdín ar chúl (sonraí tugtha) a shocrú le hardú nó le fál.

To ask the Chief Executive to arrange the garden wall of the rear of (**details supplied**) to be raised or fenced

CHIEF EXECUTIVE'S REPLY:

De réir Lámhleabhar Tionóntaí Chomhairle Cathrach Bhaile Átha Cliath (Lch. 7) is ar an tionónta atá an fhreagracht as ballaí gairdíní, geataí, sconsaí, ráillí, fála agus teorainneacha gairdíní eile.

In accordance with Dublin City Council's [Tenant Handbook](#) (Pg. 7) responsibility for garden walls, gates, fences, railings, hedges and other garden boundaries rests with the tenant.

Q.89 COUNCILLOR COLM O'ROURKE

To ask the Chief Executive that the Bottle Bank at **(details supplied)** be monitored by the Litter Warden, be regularly cleaned apart from the collections at the bank, that the environs be improved and that CCTV be installed here in the future to deal with the issues here.

CHIEF EXECUTIVE'S REPLY:

The bring bank facility at **(details supplied)** is a very problematic site. Due to its remote location, it is prone to high levels of illegal dumping. In addition to regular servicing by glass and textile recycling service providers, the site is also visited on a daily basis by the Area Cleansing team to remove any illegally dumped items. Unfortunately, illegal dumping continues after the area is cleaned and it may be up to 24 hours before Cleansing staff return to the site. In addition to the daily visits by Area based Cleansing staff, Waste Management Services is currently engaged in a pilot bottle bank maintenance program in the north city, and a new Bottle Bank Maintenance Team has been established. We have identified the most problematic sites in the north city and established a schedule whereby ten such sites are visited each day, and each site is visited several times per week. **(details supplied)** is one such site.

The use of CCTV for the purposes of Prevention, Detection and Prosecution of litter offences has been affected by changes in legislation introduced in the Circular Economy and Miscellaneous Provisions Act 2022, and of use CCTV for these purposes is not currently possible. While the Code of Practice for the operation CCTV cameras for the purposes of Prevention, Detection and Prosecution of litter offences has very recently been agreed by the relevant Departments and agencies, it must be noted that while the relevant legislation and commencement date has been signed into law, there is still a large body of work that needs to be done in preparation for the deployment of CCTV cameras. Dublin City Council's Litter Prevention Officer is currently working with the Waste Enforcement Regional Lead Authority office who are preparing a suite of documents to assist Local Authorities in preparing Data Protection Impact Assessments (DPIA's) and CCTV schemes.

An Oversight Body must be set up and all of the above will ensure that Dublin City Council will have undertaken a "privacy by design and default" process prior to the deployment of CCTV. It is Dublin City Councils intention to proceed to tender for a contractor to provide CCTV services. It is envisaged that all of the above will take some time.

I have requested the Litter Warden Service Manager to direct his staff to regularly monitor the bring bank facility on Ballybogan Road.

Q.90 COUNCILLOR COLM O'ROURKE

To ask the Manager that the road at **(details supplied)** be resurfaced. There are dangerous potholes here and the road surface is in an extremely poor condition. Furthermore, could a timeline be given as to when this road surface can be repaired?

CHIEF EXECUTIVE'S REPLY:

Road Maintenance has carried out an inspection of the condition **(details supplied)** and carried out a localised potholes repairs and will continue to do the same if any defects arise. In addition, the location is already on our list to be considered for future works programmes should the funds become available.

Q.91 **COUNCILLOR COLM O’ROURKE**

To ask the Chief Executive that speed ramps as requested by residents be installed at **(details supplied)** and that other measures be also put in place to deal with speeding at this location.

CHIEF EXECUTIVE’S REPLY:

Recently a speed survey was conducted on **(details supplied)**. The resultant 85% percentile speed was found to be appropriate and have not indicated the presence of speeding vehicles which suggests that the road is compliant with the speed limit. Traffic volumes are low and do not meet the warrant for traffic calming.

The road is narrow and not conducive to speed. It was observed that vehicles have been illegally parked on the footpath. **(Details supplied)** is wide enough for 1 side parking. On street parking naturally traffic calm the roads by reducing the carriageway width and raising driver’s attention.

Q.92 **COUNCILLOR COLM O’ROURKE**

To ask the Chief Executive that the trees be pruned at **(details supplied)**

CHIEF EXECUTIVE’S REPLY:

Parks services have listed the above streets for consideration and inclusion in our city wide tree survey programme. Any recommendations emerging from this report will inform future pruning works.